

COOK AVENUE (C-AIR HOUSING DEVELOPMENT)
STRUCTURE PLAN

Structure Plan No 5

This Structure Plan is prepared under the provisions of Part 9 of the City of Joondalup District Planning Scheme No 2

CERTIFICATION OF AGREED STRUCTURE PLAN

(SCHEDULE 8)

CERTIFIED THAT AGREED STRUCTURE PLAN 5 COOK AVENUE (C-AIR HOUSING DEVELOPMENT) STRUCTURE PLAN WAS ADOPTED BY RESOLUTION OF THE WESTERN AUSTRALIAN PLANNING COMMISSION ON

.....

being an officer of the Commission duly authorised by the Commission pursuant to section 57 of the *Western Australian Planning Commission Act 1985*

AND BY RESOLUTION OF THE COUNCIL OF THE CITY OF JOONDALUP ON AND THE SEAL OF THE CITY OF JOONDALUP WAS PURSUANT TO THE COUNCIL'S RESOLUTION HERETO AFFIXED IN THE PRESENCE OF

.....

Mayor

.....

Chief Executive Officer

**Record of Amendments made to the Agreed
Cook Avenue (C-Air Housing Development) Structure Plan**

Amendment No.	Description of Amendment	Endorsed by Council	Endorsed by WAPC

CONTENTS

<i>1</i>	<i>OVERVIEW</i>	
<i>1.1</i>	<i>STATUTORY BASIS</i>	<i>1</i>
<i>1.2</i>	<i>SUMMARY</i>	<i>1</i>
<i>2</i>	<i>STRUCTURE PLAN - PART 1</i>	
<i>2.1</i>	<i>STATUTORY WEIGHT</i>	<i>2</i>
<i>2.1.1</i>	<i>SUBJECT AREA</i>	<i>2</i>
<i>2.2</i>	<i>PRECINCTS</i>	<i>2</i>
<i>2.3</i>	<i>DEFINITIONS</i>	<i>2</i>
<i>2.4</i>	<i>THE SCHEME</i>	<i>3</i>
<i>2.5</i>	<i>RESIDENTIAL DESIGN CODES</i>	<i>3</i>
<i>2.6</i>	<i>TOWER ELEMENTS</i>	<i>3</i>
<i>2.7</i>	<i>RESIDENTIAL DESIGN</i>	<i>4</i>
<i>2.8</i>	<i>TRAFFIC MANAGEMENT</i>	<i>5</i>
<i>2.8.1</i>	<i>PARKING</i>	<i>5</i>
<i>2.8.2</i>	<i>TRAFFIC MEASURES</i>	<i>5</i>
<i>2.9</i>	<i>PROVISIONS</i>	<i>6</i>
<i>2.10</i>	<i>PERIMETER HOUSING PRECINCT</i>	<i>6</i>
<i>2.10.1</i>	<i>OBJECTIVES</i>	<i>6</i>
<i>2.10.2</i>	<i>CRITERIA</i>	<i>6</i>
<i>2.11</i>	<i>INTERNAL HOUSING PRECINCT</i>	<i>7</i>
<i>2.11.1</i>	<i>OBJECTIVES</i>	<i>7</i>
<i>2.11.2</i>	<i>CRITERIA</i>	<i>8</i>
<i>2.12</i>	<i>MULTIPLE HOUSING PRECINCT</i>	<i>9</i>
<i>2.12.1</i>	<i>OBJECTIVES</i>	<i>9</i>
<i>2.12.2</i>	<i>CRITERIA</i>	<i>9</i>

PART 1

1 OVERVIEW

1.1 STATUTORY BASIS

Clause 9.8 of the City of Joondalup District Planning Scheme No. 2 (hereinafter called "the Scheme") provides, amongst other things that a provision, standard or requirement of a Structure Plan approved under Part 9 of the Scheme shall be given the same force and effect as if it was a provision, standard or requirement of the Scheme. Furthermore, Clause 9.8.3(g) of the Scheme states that an agreed structure plan may distinguish between provisions, requirements or standards which are intended to have effect as included in the Scheme, and those that are not. It is hereby provided that such force and effect shall only be given to Part 1 of this Structure Plan. Part 2 of this Structure Plan is for explanatory purposes only, providing a descriptive analysis of the structure plan.

Subclause 9.8.3 (f) of the Scheme, states that, where, in the event of there being any inconsistency or conflict between any provision, requirement or standard of the Scheme and any provision, requirement or standard of an Agreed Structure Plan, the provision, requirement or standard of the Scheme shall prevail.

1.2 SUMMARY

The Cook Avenue (C-Air Housing Development) Structure Plan refers to Lot 124, Cook Avenue in the suburb of Hillarys, Swan location 16388 (Vol. 1902, folio 554). This lot has an area of 3.99964 hectares.

The site is reserved as Local Reserve: Public Purpose - Primary School under the Scheme for primary school purposes. A scheme amendment is proposed that would see this land zoned to Urban Development Zone, with a low to medium density residential coding.

This Structure Plan provides the framework for the progressive development of the subject land. The area is divided into three residential precincts, each of which has different design elements to ensure a variety of residential housing is developed.

2 STRUCTURE PLAN - PART 1

2.1 STATUTORY WEIGHT

As provided for under the provisions of Clause 9.8.3, and Part 9 of the Scheme, this part of the Structure Plan has the same force and effect as a provision, standard or requirement of the Scheme.

Part 1 of this Structure Plan has been endorsed by the Western Australian Planning Commission (WAPC) as a general guide to future subdivision, zoning and development of the land included in the *Cook Avenue (C-Air Housing Development) Structure Plan* area.

If a variation to the Structure Plan criteria is sought, planning approval must be sought by way of a development approval application to the Council.

2.1.1 Subject Area

The Cook Avenue (C-Air Housing Development) Structure Plan area comprises the sum of Lot 124, being approximately 4 hectares of Swan location 16388, Cook Avenue, Hillarys. The Certificate of Title is contained in *Annex A* for reference.

2.2 PRECINCTS

Plan 1 - 'Structure Plan Map' - indicates the Residential Precincts for the subject land described in the text. This map defines the following Precincts:

- Perimeter Housing Precinct;
- Internal Housing Precinct; and
- Multiple Housing Precinct.

2.3 DEFINITIONS

The terms used in this Structure Plan shall have the interpretations set out hereunder:

"BUILDING HEIGHT" means the vertical distance at any point from natural ground level to the uppermost part of the building above that point (roof ridge, parapet or wall), excluding minor projections above that point.

"GROUND LEVEL" means the finished level of lots re-contoured as part of subdivisional works by the developer and as shown on a survey of the lot, which is to be submitted together with a building application.

"STOREY" shall mean the vertical space extending from one habitable floor of a building to the floor above and for residential properties shall be deemed to be no more than 3.5 metres. The term shall not include any space within a roof, whether used for habitation or not.

"THE SCHEME" shall mean the City of Joondalup District Planning Scheme No 2 (as amended) gazetted 28 November 2000 or such amendments or modifications thereto that may be current.

2.4 *THE SCHEME*

Unless provided for by specific requirements in this Structure Plan, all requirements shall be in accordance with the City of Joondalup District Planning Scheme No 2 (as amended) gazetted 28 November 2000 or such amendments or modifications thereto that may be current.

2.5 *RESIDENTIAL DESIGN CODES*

All dwellings, as indicated on the Indicative Building Footprint Plan (Plan 3), are required to comply with the Acceptable Development provisions (ADP's) of the Residential Design Codes of Western Australia (the Codes), unless otherwise provided for by the specific requirements of this Structure Plan. Any proposed development that deviates from the ADP's will be required to address the Performance Criteria of the Codes by way of an application for development approval to the City of Joondalup.

Pursuant to Clause 4.2.5 of the Scheme, Plan 1 - 'Structure Plan Map' - identifies the location of the R25 and R40 residential densities that apply to the site.

2.6 *TOWER ELEMENTS*

Tower elements are encouraged on corner lots of the Perimeter and Internal Housing Precincts, and at the entrances to the C-Air Housing Development. Tower elements must be consistent with the design provisions described for each Precinct, with a variation from the specified design provisions requiring the tower element to be approved as the subject of a development application. Any application for development approval shall be accompanied by an explanation of the likely impact of the height and location of the tower

element on the amenity of surrounding areas and adjacent residential lots, having particular regard to:

- (i) overshadowing and sun-shading; and
- (ii) the protection of privacy for private outdoor areas and internal spaces.

The locations of potential tower elements are demonstrated on Plan 3 - 'Indicative Building Footprint Plan'.

2.7

RESIDENTIAL DESIGN

The aim of each Precinct is to create a distinguishable built form character through design and materials. It is also the intention to create a sustainable housing development that makes the most of aspects such as solar access, pedestrian accessibility, and neighbourhood amenity. Plan 2 - 'Development Layout Plan' illustrates the layout of the overall subdivision.

Plan 4 - 'Indicative Streetscape and Section Plan' indicates the street elevation from the Public Open Space looking west and the cross section from Willandra Drive to the north. It indicates the finished floor levels of the indicative dwellings.

Solar Access

Where subdivisional design constraints allow, passive solar access can be achieved through the northerly orientation of daytime living areas and windows. It is the intention at the C-Air Housing development to achieve passive solar access as part of the design, in order to fulfil acceptable development criteria.

Building Bulk

With zero lot line development permitted, it is important to ensure that houses are not designed with large expanses of blank wall. It is intended that houses be designed with windows overlooking public spaces and roads.

Willandra Street Frontages

Proposed housing fronting Willandra Street is to be of single storey height to acknowledge the views of residents' in existing properties in that street. Furthermore, only one boundary to each property can be of zero lot line to create a break in development.

Roof Pitch

Roof pitches will be no less than 26 degrees and no greater than 35 degrees across all precincts.

Materials

All houses will be constructed with non-reflective materials such as masonry (rendered or brick finish), roof tiles and / or Colorbond roofing. Colours will vary across the precincts.

2.8 *TRAFFIC MANAGEMENT*

2.8.1 *Parking*

Investa has provided on-street parking for visitors in various locations around the site. There are 53 on-street visitor car parks provided internally to the site (note, parking is possible on both sides of the main east-west internal access). In addition, on-street parking is available in Willandra Drive, New England and Ferndene Mews (it is estimated there are at least 48 car parks available along New England and Willandra Drives alone). In addition, each lot has 2 car parks provided on-site.

2.8.2 *Traffic Measures*

The local residents have concerns about the existing traffic speeds along Cook Avenue. In order to address this existing issue and to accommodate proposed vehicle access to the site appropriate road widths to Cook Avenue, inclusive of a bike lane and dual use path, Investa have undertaken to make alterations to that Avenue. Accordingly, the proposed alterations to Cook Avenue are to be detailed in a management report as part of the subdivision application process. The measures will include:

- ☐ creation of a pavement suitable to accommodate a single carriageway on both sides of the road inclusive of a bike lane;
- ☐ inclusion of a dual use path in the road reserve;
- ☐ provision of a centralised median to tighten the pavement and reduce speeds along Cook Avenue;
- ☐ inclusion of trees in the medians to add a vertical element to assist in speed reduction; and
- ☐ location of proposed crossovers to acknowledge the location of existing driveways to the existing residential properties to the north.

2.9

PROVISIONS

This document identifies three precincts, distinguished by built form character. Together these precincts will provide a diversity of housing choice and lifestyle options for residents choosing to locate there. For each precinct, provisions include *objectives* (which describe in general terms the intentions to be addresses in each precinct); and *criteria* (which set out built form requirements ("shall..") and preferred treatments ("should...")).

2.10

PERIMETER HOUSING PRECINCT

2.10.1

Objectives

Objectives intended for the Perimeter Housing Precinct include:

- (i) Encourage a variety of single residential housing types;
- (ii) Promote layouts that provide for pedestrian-friendly streetscapes with passive surveillance of the public spaces;
- (iii) Encourage the creation of a consistent form and character through the consistent use and combination of appropriate housing design, colours and material; and
- (iv) Ensure that development has regard for the amenity of adjacent lots and surrounding areas.

2.10.2

Criteria

Criteria to be satisfied in this precinct are:

- (i) Notwithstanding the provisions of the Residential Design Codes, buildings should achieve a setback from the front boundary as follows: *dwellings* - 3m; *garages* - 4.5m; *rear* - nil;
- (ii) Each house must provide at least 2 covered car-bays on site;
- (iii) In order to maximise site usage, zero lot line development is permitted to one site boundary;
- (iv) Buildings on corners shall address each street frontage and the corner with equal importance;
- (v) Buildings shall be designed to have active frontages with no blank facades fronting the street or public open space;

- (vi) Advertising, signage or hoardings will not be permitted except in conformity with the provisions of the City of Joondalup Signs Local Law for a residential area;
- (vii) Roofs: Where pitched, roofs shall be no less than 26 degrees and no greater than 35 degrees. Minor projections such as verandahs and canopies may have shallower pitches. Curved roofs and flat roofs concealed by parapets shall also be permitted;
- (viii) Utility areas such as bin and service areas shall be screened from view from streets and public open spaces;
- (ix) The provisions of City of Joondalup Policy 3.1.9, "Height and Scale of Buildings within a Residential Area" shall not apply;
- (x) The maximum Building Height measured from Ground Level shall be as follows: *wall height* - 7m; *roof ridge* - 11m;
- (xi) Houses fronting Willandra Drive shall be single storey at street level;
- (xii) Notwithstanding the provisions of the Residential Planning Codes, private open space shall not be less than 40% of each individual lots; and
- (xiii) Tower elements with plan dimensions no greater than 4m by 4m may exceed the building height requirements but air conditioners, solar panels and satellite dishes may not. A tower element must contribute to the landmark quality of the building, and can take the form of towers, lookouts, and lofts. Tower elements can only be located on corner lots and at the entrance to the C-Air Housing Development.

2.11 INTERNAL HOUSING PRECINCT

2.11.1 Objectives

Objectives intended for the Internal Housing Precinct include:

- (i) Encourage a diversity of housing types;
- (ii) Encourage a strong sense of architectural identity at special locations, such as opposite public open space and corner sites; and
- (iii) Ensure that development has regard for the amenity of adjacent lots and surrounding areas.

2.11.2

Criteria

Criteria to be satisfied in this precinct are:

- (i) Notwithstanding the provisions of the Residential Planning Codes, buildings should achieve a setback from boundaries as follows: *front*: dwelling - 3m; garages - 1.5m; *Side*: nil setbacks to both boundaries is permitted (provided Clause (b) below is satisfied); *Rear*: nil setback is permitted;
- (ii) Zero lot line development will be permitted as set out in Clause (a) above, provided that any overshadowing of adjoining lots does not exceed 50% of the private outdoor living area;
- (iii) Carparking shall be accessed from rear lanes where they are provided. Each house must provide 2 onsite car parking spaces, of which at least 1 must be covered;
- (iv) Where lots front onto public open space and notwithstanding the provisions of the Residential Planning Codes, the dwelling shall be set back 2m to the open space boundary;
- (v) Buildings on corners shall address each street frontage and the corner with equal importance;
- (vi) Buildings shall be designed to have active frontages with minimal blank facades fronting the street or public open space;
- (vii) Advertising, signage or hoardings will not be permitted except in conformity with the provisions of the City of Joondalup Signs Local Law for a residential area;
- (viii) Roofs: Where pitched roofs shall be no less than 26 degrees and no greater than 35 degrees. Minor projections such as verandahs and canopies may have shallower pitches. Curved roofs and flat roofs concealed by parapets shall also be permitted;
- (ix) Utility areas such as bin and service areas shall be screened from view from streets and public open spaces;
- (x) The provisions of City of Joondalup Policy 3.1.9 "Height and Scale of Buildings within a Residential Area" shall not apply;
- (xi) The maximum Building Height measured from Ground Level shall be as follows: *wall height* - 7m; *roof ridge* - 11m;
- (xii) Notwithstanding the provisions of the Residential Planning Codes, private open space shall not be less than 40% of each individual lot; and

- (xiii) Tower elements with plan dimensions no greater than 4m by 4m may exceed the building height requirements but air conditioners, solar panels and satellite dishes may not. A tower element must contribute to the landmark quality of the building, and can take the form of towers, lookouts, and lofts. Tower elements can only be located on corner lots and at the entrance to the *C-Air* Housing Development.

2.12 *MULTIPLE HOUSING PRECINCT*

2.12.1 *Objectives*

Objectives intended for the Multiple Housing Precinct include:

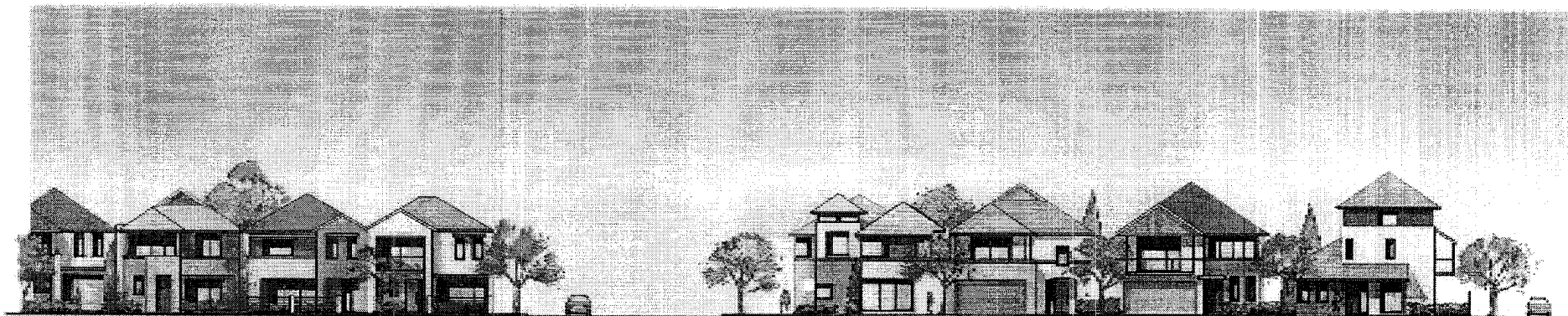
- (i) Encourage a diversity of housing types;
- (ii) Encourage a strong sense of architectural identity at special locations, such as opposite public open space; and
- (iii) Ensure that development has regard for the amenity of adjacent lots and surrounding areas.

2.12.2 *Criteria*

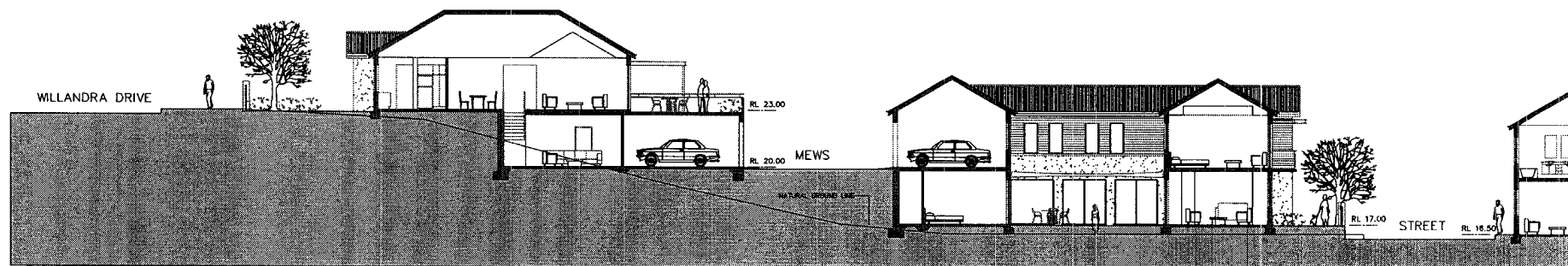
Criteria to be satisfied in this precinct are:

- (i) Notwithstanding the provisions of the Residential Planning Codes, buildings should achieve a setback from boundaries as follows: *front* - 6m; *public open space* - 3m; *public access way* - 1.5m;
- (ii) Each residential dwelling must provide 2 onsite car parking spaces, of which at least 1 must be covered;
- (iii) Buildings shall be designed to have active frontages with minimal blank facades fronting the street or public open space;
- (iv) Advertising, signage or hoardings will not be permitted except in conformity with the provisions of the City of Joondalup Signs Local Law for a residential area;
- (v) Roofs: Where pitched roofs shall be no less than 26 degrees and no greater than 35 degrees. Minor projections such as verandahs and canopies may have shallower pitches. Curved roofs and flat roofs concealed by parapets shall also be permitted;
- (vi) Utility areas such as bin and service areas shall be screened from view from streets and public open spaces;

- (vii) The provisions of City of Joondalup Policy 3.1.9 "Height and Scale of Buildings within a Residential Area" shall not apply;
- (viii) The maximum Building Height measured from Ground Level shall be as follows: *wall height* - 9m; *roof ridge* - 13m;
- (ix) Notwithstanding the provisions of the Residential Planning Codes, a minimum of 16m² of balcony space for each residential dwelling shall be provided in lieu of communal open space; and
- (x) No tower elements are permitted in this precinct.



Elevation from Public Open Space Looking West



Western Cross Section

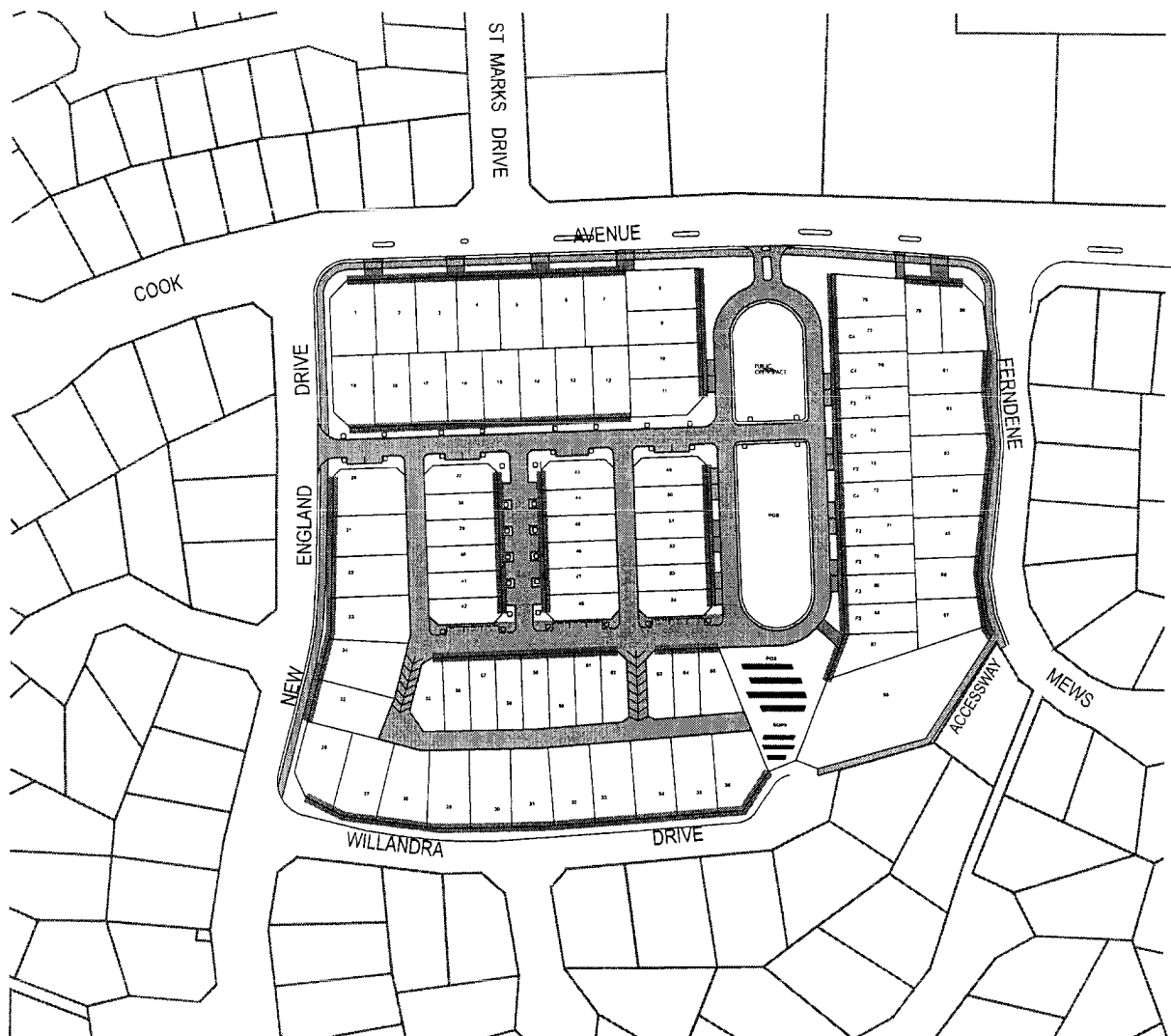
Note: All levels are subject to final engineering design.







- Tower Elements



Plan 3: Indicative Building Footprint Plan

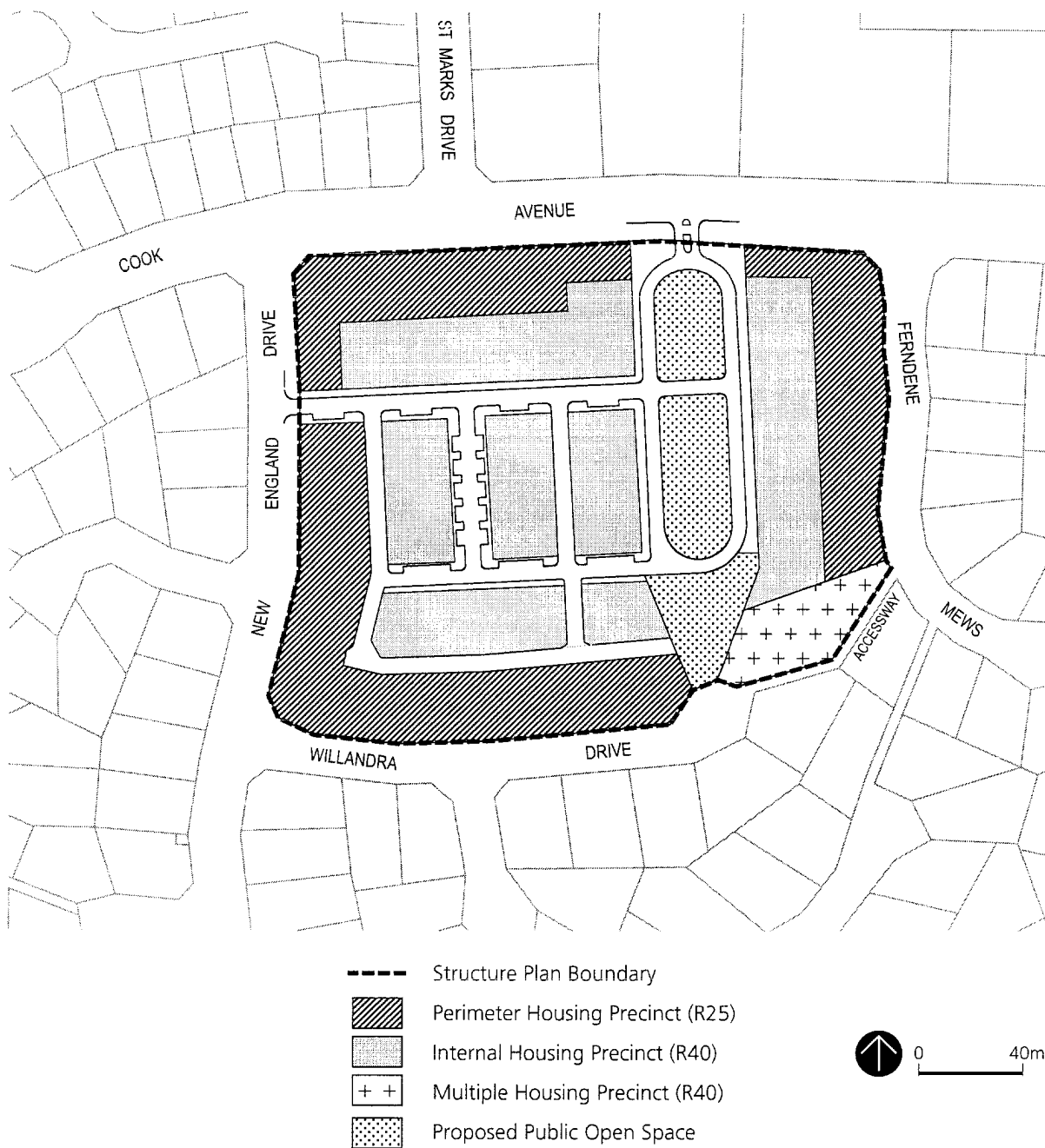


LEGEND

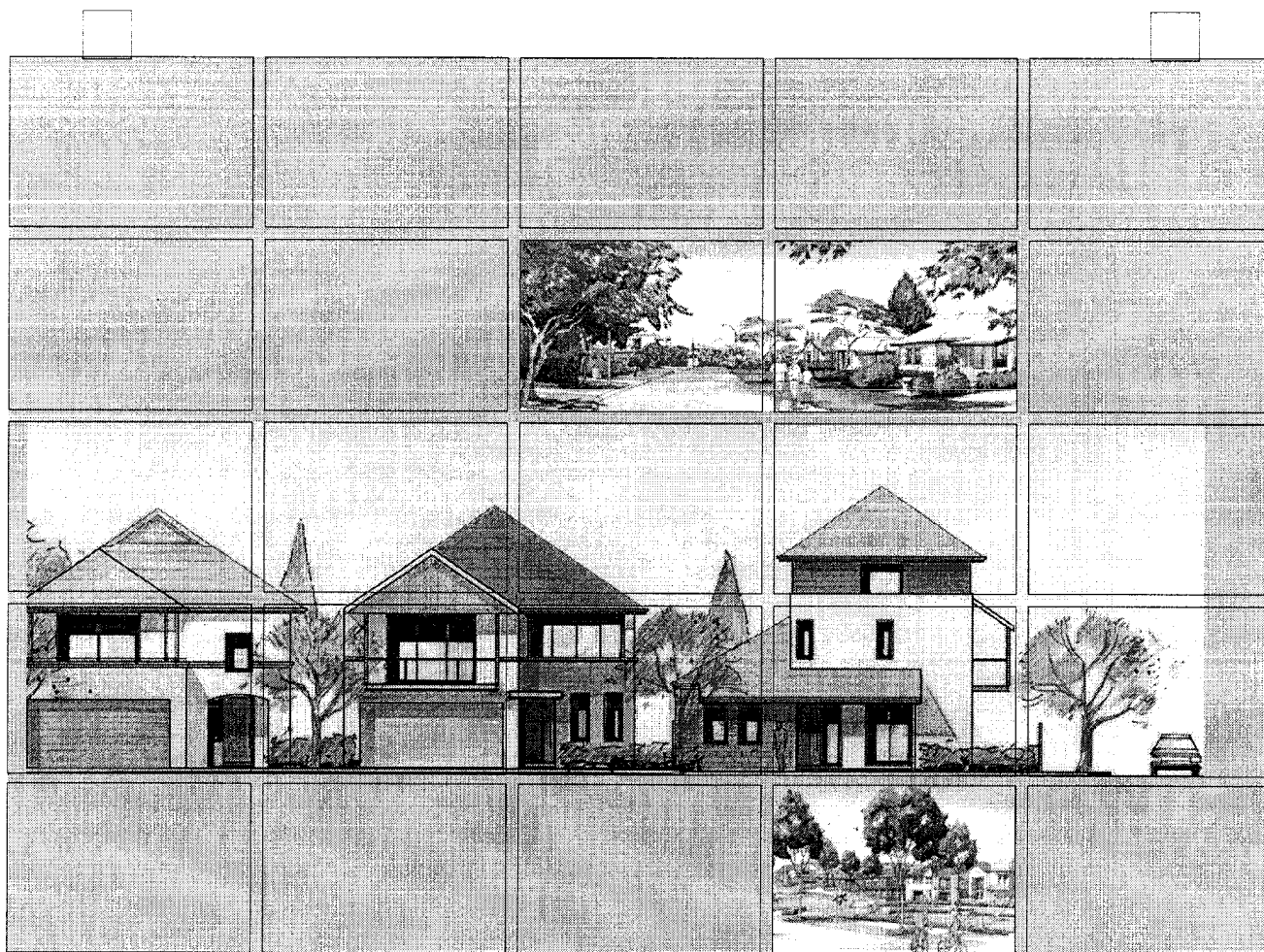
-  Dual Use Path 2.4m wide
-  Future Crossover
-  Road Pavement
-  Frontage of Lots



Plan 2: Development Layout Plan



Plan 1: Structure Plan Map



Proposed C-Air Townhouse Development Structure Plan

This Structure Plan is prepared under the provisions of Part 9 of the
City of Joondalup District Planning Scheme No. 2

