

## Attachment 2

### PART ONE : PROPOSED STRUCTURE PLAN

#### 1.0 STRUCTURE PLAN AREA

The Structure Plan area comprises 34 hectares bounded by Joondalup Drive to the east, Hodges Drive to the north west, the Mitchell Freeway to the west and south west and Eddystone Avenue to the south.

#### 2.0 STRUCTURE PLAN PRECINCTS AND LAND USE DESIGNATIONS

The Structure Plan shows the road structure and divides the structure plan area into the following precincts and land uses:

- Bulk Retail/showroom Precinct
- Bulk Retail/Showroom and/or Technology Park Precinct
- Service Industry Precinct
- Bulk Retail/Showroom/Service Industry Precinct
- Drainage Purpose Precincts

#### 3.0 INTERPRETATIONS

The words and expressions used in this Structure Plan shall have the respective meanings given to them in the City of Joondalup District Planning Scheme No.2 (the Scheme). Where not listed in the Scheme the following interpretations shall apply.

**'Architectural Elements'** means elements built into the design and construction of a building to enhance its style, presentation and variety and may include tower elements, spines, gables, mouldings, awnings, verandahs, etchings and any other built form element.

**'Car Parking Levels'** means the ground level of areas provided for the purposes of car parking. Such areas shall have a maximum grade of 5% with ramps providing access to car parking areas to have a maximum grade of 10%.

**'Ground Floor Level'** means the floor level of a building +/- 0.5 metres from the finished Australian Height Datum (AHD) level of an existing adjacent road that the building fronts or the future level of the lot created by a future subdivision approval granted by the Western Australian Planning Commission over the land subject to this structure plan.

**‘Masonry’** means a building and construction finish which includes a palette of materials of a masonry base and shall include materials such as stone, concrete and brickwork but does not include materials such as metal, timber, fibro cement sheeting or other similar material.

**‘Research Facility’** means premises used for the purposes of conducting investigations by means of scientific or technological research and may include offices, laboratories and ancillary facilities.

**‘Municipal Depot Facility’** means premises used for the maintenance and garaging of vehicles, storage of materials and administration of tasks, associated with the provision of services to the municipality by the City of Joondalup and includes all activities ancillary to this function.

#### **4.0 THE SCHEME**

Unless provided for by specific requirements of this Structure Plan, all requirements shall be in accordance with the provisions of the Scheme.

#### **5.0 FUTURE SUBDIVISION**

Future subdivision application(s) will be required to be considered by the City over the structure plan area. The future subdivision of the site and corresponding engineering drawings submitted to the City must achieve, where practical and with the City’s approval, retention and protection of significant remnant native trees upon the site. Where possible, such trees should be protected through variation to the alignment of proposed roads so that trees are located within proposed road reserves, within 3 metres to any street lot boundary (so that they can be incorporated into any future landscaping associated with the development of the lot), traffic islands, roundabouts and proposed drainage sites.

The subdivision of the land shall also ensure that future lot levels created are sympathetic to the adjacent road level. In this regard, the finished lot level shall be +/- 0.5 metres from the finished Australian Height Datum (AHD) level of the adjacent road that the lot fronts, as noted in Part 3.0 Interpretations. If dual road frontage lots or “superlots” with more than one lot frontage are proposed at the subdivision stage, the City may consider varying this requirement at this stage of development of the land, provided that the finished lot level is generally in accordance with the overall intention for it to be relative to the finished Australian Height Datum (AHD) level of the adjacent road(s).

## **6.0 BULK RETAIL/SHOWROOM PRECINCT**

### **6.1 Objectives**

The bulk retail/showroom precinct is intended to accommodate whole-saling, warehouses, showrooms and trade and professional services, as well as providing for large scale commercial business and large scale category/theme based bulk retail outlets.

The objectives of the bulk retail/showroom precincts are to:

- a) Provide for retail and commercial businesses which require large areas such as bulky goods and large scale category/theme based retail outlets as well as complementary business services.
- b) Ensure that development within this precinct creates an attractive façade to the street and provides visual amenity for surrounding areas and as a southern gateway to the Joondalup City Centre.
- c) Ensure development within this precinct can be progressed in an efficient, coordinated and flexible manner.
- d) Promote the application of sustainable and energy efficient building and design techniques to developments within this precinct.

### **6.2 Permitted Uses**

The following land uses are permitted in the Bulk Retail/Showroom Precinct, subject to any conditions that the Council may wish to impose in granting its approval and compliance with the provisions of this Structure Plan.

- Amusement Facility/Parlour
- Art Gallery
- Auction Room
- Bakery
- Bank
- Car Park
- Car Wash
- Child Care Centre
- Civic Building
- Hardware Store
- Kindergarten
- Laundrette
- Laundry
- Market (Retail)
- Medical Centre
- Office
- Open Air Display
- Private Recreation

- Club (Non-Residential)
- Communication Antenna
- Costume Hire
- Dry Cleaning Premises
- Education Establishment
- Equipment Hire
- Funeral Parlour
- Garden Centre
- Hall
- Place of Assembly
- Place of Workshop
- Public Exhibition Facility
- Reception Centre
- Recreation Centre
- Showroom
- Trade Display
- Vehicle Sales/Hire Premises
- Veterinary Consulting Rooms
- Veterinary Hospital
- Warehouse

### 6.3 Provisions

#### 6.3.1 Setbacks

In the Bulk Retail/Showroom Precinct setbacks shall be as follows:

(i) Joondalup Drive : 70% of the width of the front façade of the building shall be set back a minimum of 15m and a maximum of 25m. Where the maximum 25m setback is applied, a minimum area of 2 metres in depth shall be provided for uses other than vehicle access or parking (for example, pedestrian access, display purposes or landscaping).

(ii) Eddystone Avenue : 70% of the width of the front façade of the building shall be set back a minimum of 15m and a maximum of 25m. Where the maximum 25m setback is applied, a minimum area of 2 metres in depth shall be provided for uses other than vehicle access or parking (for example, pedestrian access, display purposes or landscaping)

- (iii) Mitchell Freeway : The minimum setback to the Mitchell Freeway shall be the eastern edge of the Western Power easement parallel to the Mitchell Freeway.
- (iv) Other roads : The minimum setback from other roads shall be 6m from the primary street (street to which the building fronts) and a minimum of 3m from the secondary street.
- (v) Side and Rear Boundaries : Setbacks to side and rear boundaries can be reduced to nil provided such walls comply with the Building Code of Australia.

### 6.3.2 *Building Orientation*

All buildings shall be oriented to the road that the lot upon which the building is proposed, fronts. Where a lot is located at the intersection of Joondalup Drive and another road then buildings within that lot shall be oriented towards Joondalup Drive.

### 6.3.3 *Landscaping*

In the Bulk Retail/Showroom Precinct a minimum 8% of the subject site shall be provided as landscaping with a minimum width of 3m along any street frontage. Landscaping shall incorporate any remnant vegetation protected as part of the approved subdivision of the land and largely be comprised of native species, in particular those species native to the locality. The use of exotic species may also be considered, however shall be limited to areas where the use of native vegetation is not suitable (ie heavily shaded areas).

A detailed landscape plan shall be submitted to the City for building license approval.

A minimum of 1 shade tree per 4 car parking bays shall also be required.

#### 6.3.4 *Car Parking*

In the Bulk Retail/Showroom Precinct car parking shall be provided in accordance with the relevant Scheme standard for the proposed use, as denoted in Table 2 of the Scheme. Where no Scheme standard is specified, 1 car bay for every 30m<sup>2</sup> Net Lettable Area (NLA) is to be provided.

#### 6.3.5 *Access*

In the Bulk Retail/Showroom Precinct reciprocal rights of access shall be provided as shown on the Structure Plan along the Joondalup Drive and Eddystone Avenue frontages.

Where minimum setbacks are adopted, carparking bays shall be positioned adjacent to the front boundary such that the reciprocal access across buildings is continuous.

#### 6.3.6 *Building Height*

In the Bulk Retail/Showroom Precinct the maximum building height permitted shall be 13m other than landmark Architectural elements which may be up to a maximum 18m in height and limited in size by a maximum floor area of 80m<sup>2</sup>.

#### 6.3.7 *Materials and Finishes*

In the Bulk Retail/Showroom Precinct the following minimum standard of materials and finish shall be required.

- (i) All street facades shall be constructed in a masonry material with a minimum of 50% of the façade to incorporate glass finishes. Where concrete tilt-up panelling is proposed this shall only be permitted on the street facades of buildings when provided with a textured paint, articulated or detailed finish or combination thereof.
- (ii) That part of any street façade other than the glass finish component is to incorporate other architectural elements to enhance design and visual appearance.
- (iii) Reflective glazing shall not be permitted and where non reflective glazing is provided such glazing shall not be used for

any form of signage, unless in accordance with Schedule 4 – Exempted Advertising of the City’s District Planning Scheme No. 2.

- (iv) Corner lots must incorporate at least one tower element within its design. For other lots, the incorporation of a single tower element may be considered.
- (v) Side and rear facades shall be constructed of a masonry material and have a painted finish. Side and rear facades directly fronting on to areas accessible to the public shall be painted with anti-graffiti paint.
- (vi) All servicing and building infrastructure items (for example delivery areas, refuse areas, air conditioners etc) shall be screened from view from the street through building design, provision of landscaping or other built form means.

#### **6.3.8** *Boundary Fencing*

In the Bulk Retail/Showroom Precinct, where a building is located on a boundary, no fencing shall be required. Where buildings are setback from a rear or side boundary where appropriate, a minimum black chain mesh fence to a maximum height of 1.8m shall be provided.

Fencing located between a street frontage and a building shall be of permeable construction, of a high aesthetic standard and shall be a maximum of 1.8m in height. Materials such as fibro cement, zincalume or ringlock fencing shall not be permitted.

## **7.0 BULK RETAIL/SHOWROOM AND/OR TECHNOLOGY PARK PRECINCT**

### **7.1 Objectives**

The Bulk Retail/Showroom and/or Technology Park Precinct is intended to accommodate the opportunity for development as per the bulk retail/showroom precinct, as described in Part 6 above, and/or as an integrated Technology Park.

In addition to the objectives set out in Clause 6.1 above, the additional objectives of the Bulk/Retail Showroom and/or Technology Park Precinct is to:

- a) Provide recognition of the opportunity for a Technology Park Precinct to be located within the Structure Plan area while providing for alternative land uses in the event that the Technology Park option is not pursued.

## **7.2 Permitted Uses**

In addition to the permitted landuses outlined within Clause 6.2 above, the following additional landuses are also permitted in the Bulk/Retail Showroom and/or Technology Park Precinct, subject to any conditions that the Council may wish to impose in granting its approval and compliance with the provisions of this Structure Plan.

- Research Facility

A Research Facility is defined as an establishment used predominantly for the purposes of scientific, technical, economic, social or environmental research and development.

## **7.3 Provisions**

The development provisions contained within Clause 6.3 above shall also apply to this precinct.

## **8.0 SERVICE INDUSTRY PRECINCT PROVISIONS**

### **8.1 Objectives**

The Service Industry Precinct is intended to provide for a wide range of business, industrial and private recreational developments which the City may consider would be inappropriate in commercial and business areas, but which are capable of being conducted in a manner which will prevent them being obtrusive or detrimental to local amenity.

The objectives of the Service Industry Precinct are to:

- a) Accommodate a range of light industry, showrooms and warehouses, entertainment and private recreational activities and complimentary

business services which by their nature would not detrimentally affect the amenity of surrounding areas.

- b) Ensure that development within this precinct creates an attractive façade to the street to enhance the visual amenity of the surrounding areas.
- c) Promote the application of sustainable and energy efficient building and design techniques to developments within this precinct.

## **8.2 Permitted Uses**

Other than a bank which is not permitted in the Service Industry Precinct, the permitted landuses outlined within Clause 6.2 above are also permitted in the Service Industry Precinct, subject to any conditions that the Council may wish to impose in granting its approval and compliance with the provisions of this Structure Plan.

In addition, the following uses are permitted in the Service Industry Precinct.

- Light Industry
- Lunch Bar

## **8.3 Provisions**

The development provisions contained within Clause 6.3 above shall also apply to this precinct, with the exception of Clause 6.3.6(i) where a minimum 25% of the façade to incorporate glass finishes (in lieu of 50%).

## **9.0 BULK RETAIL/SHOWROOM/SERVICE INDUSTRY PRECINCT**

The Bulk Retail/Showroom/Service Industry Precinct is intended to provide for multiple uses including the opportunity for a combination of the Bulk Retail/Showroom and Service Industry activities identified elsewhere in the Structure Plan area. In addition, provision is made within this precinct to accommodate the development of a municipal depot site for the City of Joondalup within the Structure Plan area.

### **9.1 Objectives**

The objectives of the Bulk Retail/Showroom/Service Industry Precinct are to:

- (a) Accommodate a range of light industry, showrooms and warehouses, vehicle repairs, fuel depots, storage yard and complimentary

business services which by their nature would not detrimentally affect the amenity of the surrounding areas;

- (b) Ensure that development within this precinct creates an attractive façade to the street for the visual amenity of surrounding areas and as a gateway to the Joondalup City Centre;
- (c) Promote the application of sustainable and energy efficient building and design techniques to developments within this precinct.

## **9.2 Permitted Uses**

The permitted uses outlined within Clause 6.2 above are also permitted in the Bulk Retail/Showroom/Service Industry Precinct subject to any conditions that the Council may wish to impose in granting its approval and compliance with the provisions of this Structure Plan. In addition, the following uses are also permitted:

- Fuel Depot
- Light Industry
- Municipal Depot Facility
- Storage Yard
- Vehicle Repairs

## **9.3 Provisions**

### **9.3.1 Setbacks**

Setbacks in the Depot Site Precinct shall be as follows:

- (i) Hodges Drive : Minimum 9m.
- (ii) Mitchell Freeway : Minimum 10m

### **9.3.2 Landscaping**

A minimum 8% of the subject site shall be provided as landscaping with a minimum boundary landscaping strip of 6m width.

### **9.3.3 Car Parking**

Car parking shall be provided in accordance with the relevant Scheme standard for the proposed use, as denoted in Table 2 of the Scheme. Where no Scheme Standard is specified, 1 bay for every 50m<sup>2</sup> NLA is to be provided.

#### 9.3.4 Access

Vehicular access shall be via an internal subdivisional road. No direct access shall be permitted from the Mitchell Freeway. Left-in / Left-out access only from Hodges Drive may be permitted, subject to the approval of Main Roads Western Australia.

#### 9.3.5 Building Height

The maximum building height permitted shall be 13m other than landmark Architectural elements which may be up to a maximum 18m in height and limited in size by a maximum floor area of 80m<sup>2</sup>. In addition, the provision of any telecommunications component which shall be permitted only as part of a Municipal Depot Facility, such as masts, aerials and the like are not deemed to be building for the purposes of assessing building height.

#### 9.3.6 Materials and Finishes

The following minimum standard of materials and finish shall be required.

- (i) All street facades shall be constructed in a masonry material, with a minimum 50% of the façade to incorporate glass finishes. Should concrete tilt panel construction be utilised, it shall be painted.
- (ii) That part of any street façade other than the glass finish component is to incorporate other Architectural Elements to enhance design and visual appearance.
- (iii) Reflective glazing shall not be permitted and where non-reflective glazing is provided such glazing shall not be used for any form of signage, unless in accordance with Schedule 4 – Exempted Advertising of the City's District Planning Scheme No.2.
- (iv) Side and rear facades shall be constructed of a masonry material and have a painted finish. Side and rear facades directly fronting onto publicly accessible areas shall be painted with anti graffiti paint.

- (v) All servicing and building infrastructure items (for example delivery areas, storage areas, refuse areas, air conditioners etc) shall be screened from view from the street through building design, provision of landscaping or other built form means.

#### 9.3.7 *Boundary Fencing*

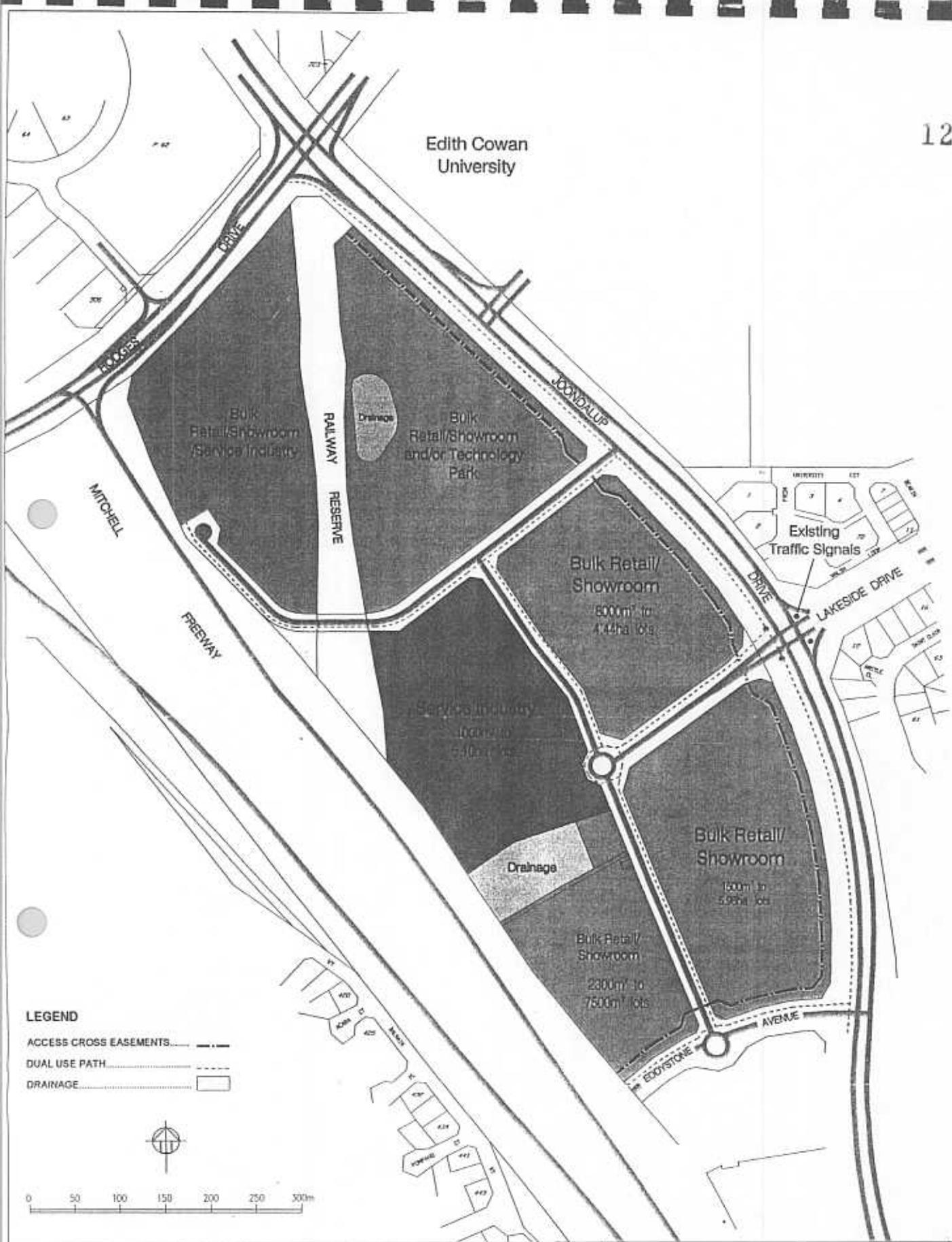
In the Bulk Retail/Showroom Precinct, where a building is located on a boundary, no fencing shall be required. Where buildings are setback from a rear or side boundary where appropriate, a minimum black chain mesh fence to a maximum height of 1.8m shall be provided.

Fencing located between a street frontage and a building shall be of permeable construction, of a high aesthetic standard and shall be a maximum of 1.8m in height. Materials such as fibro cement, zincalume or ringlock fencing shall not be permitted.

## 10.0 DRAINAGE PURPOSE PRECINCT PROVISIONS

The drainage sites identified within the structure plan are set aside to provide for a coordinated drainage response across the structure plan area. The objective of the drainage land use designation is to identify appropriate locations within the structure plan area to service drainage requirements of the proposed structure plan area **and adjacent regional road network**. All drainage sites must incorporate the protection of remnant vegetation wherever possible and be largely concealed from view wherever possible.

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LEGEND

- ACCESS CROSS EASEMENTS ————
- DUAL USE PATH ————
- DRAINAGE ————



0 50 100 150 200 250 300m

**PLAN 1**  
**STRUCTURE PLAN**  
 JOONDALUP SOUTHERN BUSINESS DISTRICT  
 CORNER HODGES DRIVE & JOONDALUP DRIVE  
 for LandCorp

SCALE: 1:4000  
 REF: L13qq 10 rps

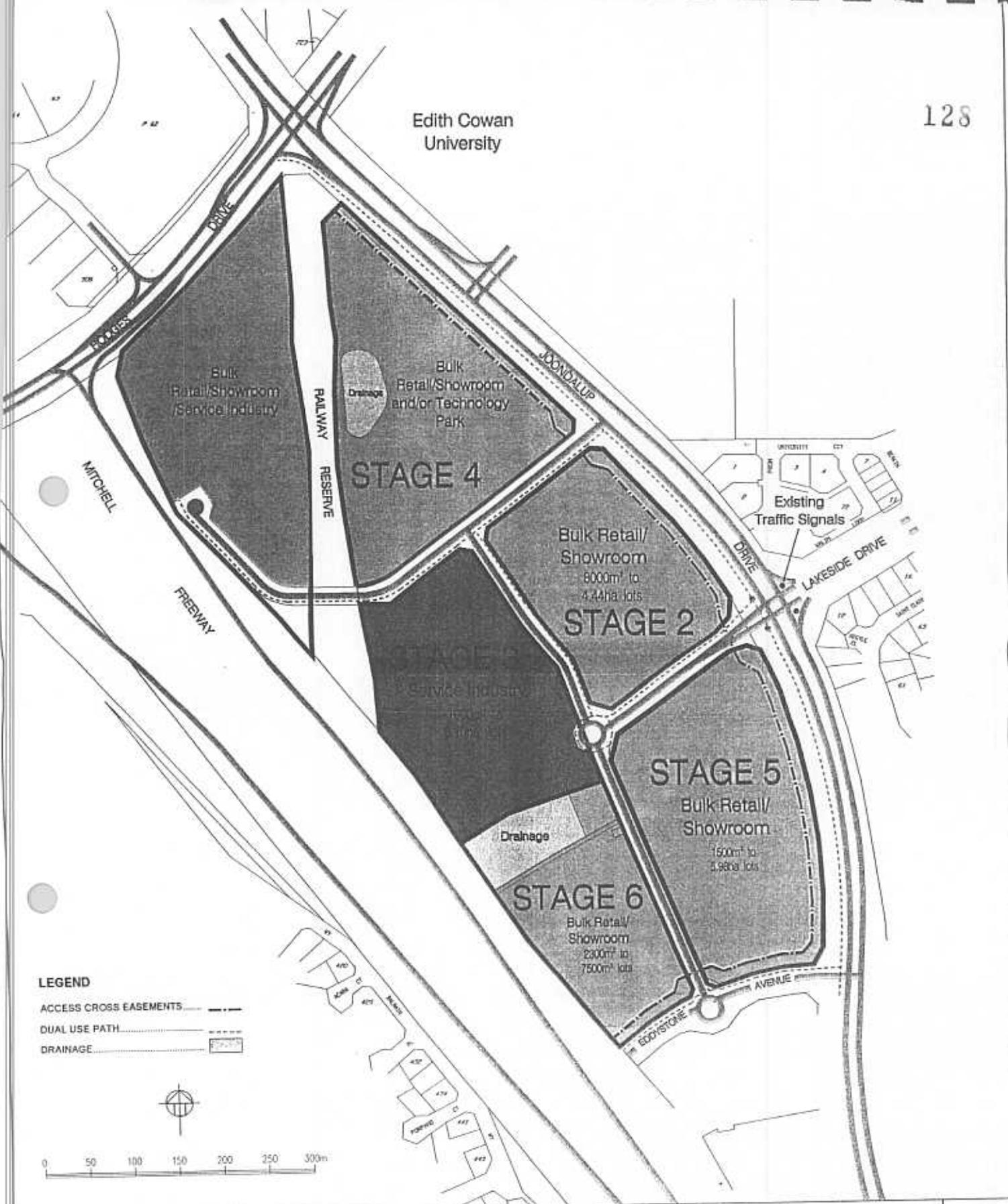
DATE: December 2003  
 Revised: 9th May 2005



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Edith Cowan University



**LEGEND**

ACCESS CROSS EASEMENTS ————

DUAL USE PATH ————

DRAINAGE ————



0 50 100 150 200 250 300m

**PLAN 2**  
**LIKELY STAGING**  
 JOONDALUP SOUTHERN BUSINESS DISTRICT  
 CORNER HODGES DRIVE & JOONDALUP DRIVE  
 for LandCorp

SCALE: 1:4000  
 REF: L13qq 13B rps

DATE: December 2003  
 Revised: 10th May 2005



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**SOUTHERN BUSINESS DISTRICT STRUCTURE PLAN  
SCHEDULE OF SUBMISSIONS FOLLOWING ADVERTISING  
(CLOSED 11 AUGUST 2005)**

NO	NAME OF SUBMITTOR	DESCRIPTION OF AFFECTED PROPERTY	SUBMISSION SUMMARY	COUNCIL'S RECOMMENDATION
1	Connolly Residents Association	N/A	Concerned about vehicular access being provided on to Hodges Drive for safety reasons. Suggests access be provided over the railway line.	The development provisions for this precinct states that vehicular access is to be achieved using the internal access road that includes a bridge over the railway. Nevertheless, left-in/left-out access only may be permitted from Hodges Drive, subject to the approval of Main Roads WA.
2	S Bosich	N/A	Objects to proposal. Concerned about greenhouse gases, global warming, loss of habitat and impacts of clearing and development on future generations.	The issues raised are not specific to the site or the structure plan process. The land is privately owned and zoned appropriately for development. Council is required to deal with any applications in accordance with the provisions of DPS2, which includes statutory timeframes.